



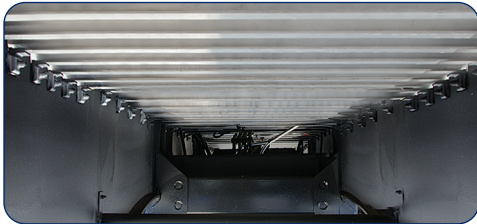
## FEATURES



### Redefining standard features

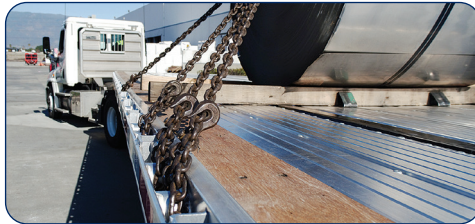
The 4000AE® combo flatbed is light weight and strong. We removed weight by using aluminum crossmembers, and changed the way the crossmembers and mainbeam work together to support the load. This re-engineered flatbed weighs less than some all-aluminum trailers, yet is available at a combo-trailer price.

#### ■ Light weight, high strength, low maintenance



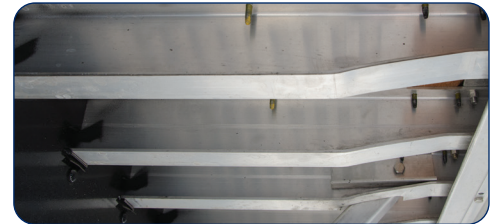
The 4000AE delivers the strength and durability of an all-steel trailer, and the low weight benefit of an all-aluminum trailer.

#### ■ 47,000 lb standard coil haul package



Designed with 3" aluminum crossbars that include nine 4" full-width, tapered aluminum crossbars on 8" centerlines to provide optimized strength at the coil.

#### ■ 3" and 4" aluminum crossmembers



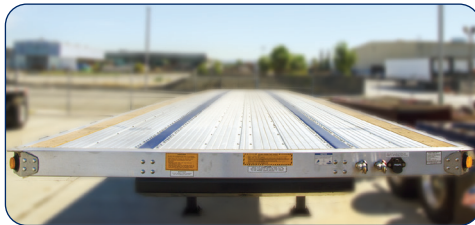
Our patented crossmembers system is featured on the 4000AE. A combination of 3" and 4" aluminum crossmembers replace steel to optimize weight without compromising strength or functionality.

#### ■ Aluminum floor with hardwood nailing strips



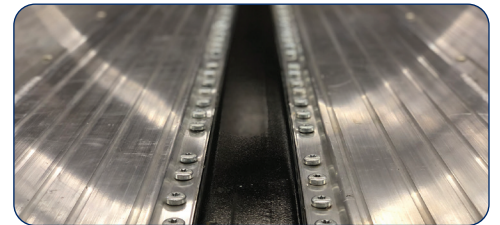
Two hardwood nailing strips are integrated into the strong aluminum deck for greater cargo securement versatility.

#### ■ Aluminum front rail



Designed to lower tare weight, this high-strength aluminum front rail provides added protection, improves durability, and reduces maintenance expenses.

#### ■ Hat-shaped top flange



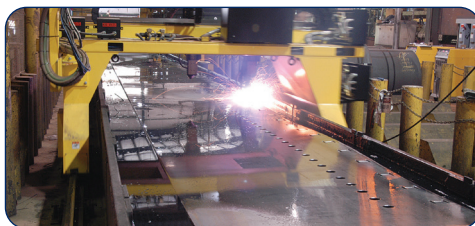
The 80K steel hat-shaped top flange provides increased lateral stability, adding to a longer trailer life.

#### ■ Heavy-duty pipe spools



The aluminum side rail has an integral winch track and heavy-duty pipe spools. The pipe spools are engineered to a 6,500 lb working load limit rating.

#### ■ Plasma-cut one-piece main beam



Camber-shaped for strength and durability, our one-piece main beams are plasma cut with absolute precision. They have less camber than more flexible all-aluminum beams and typically maintain more of their original shape when loaded. This means less load shifting and "back-slapping" as the trailer flexes up and down.

#### ■ High-quality paint process



The main beams are shot blasted to bare metal prior to painting and then coated with a two-part primer and two-part urethane top coat on the exterior surfaces. A rust preventative coating is applied to the interior surfaces of the beams for lower maintenance costs.



### ■ Integral winch track



The integral winch track is extruded into the aluminum side rails. It features a 5,000 lb working load limit rating.

### ■ Front stainless steel corners



Stainless steel corners and top plates protect against impact damage and provide additional strength and durability, reducing cost of ownership.

### ■ ConMet® PreSet Plus® premium wheel end system



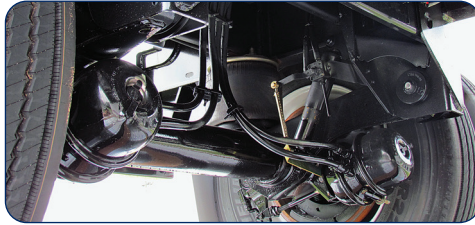
ConMet PreSet Plus includes aluminum hub, seals, bearings, spacer, and spindle nut. This premium system also comes with an 8-year warranty.

### ■ Double tapered beam at rear for proper axle loading



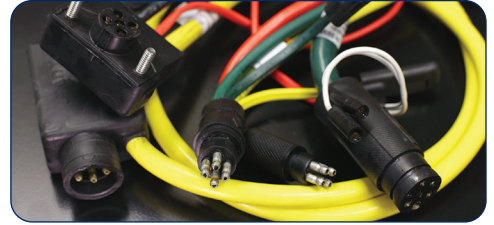
This design optimizes load distribution between the axles and extends suspension life.

### ■ Hendrickson® INTRAAX® AANT 23K 121" spread air ride suspension



Tapered beam and large-diameter axles reduce weight, minimize fatigue cracking, extend service life, and minimize maintenance costs.

### ■ 100% soldered main harness



The sealed main harness features 100% soldered internal connectors to prevent corrosive de-icing chemicals from traveling through connections into the wiring.

### ■ Heavy-duty aluminum outrigger attachments



Three tube-shaped outriggers located on each side of the flatbed are connected to the aluminum crossmembers, providing stability and helping distribute the load to the main beams.

### ■ Microencapsulated adhesive on all screws



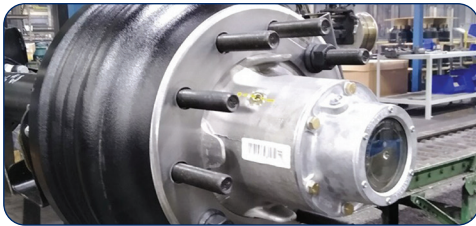
Seals and locks threaded fasteners, creating better and stronger connections. Sealed threads are vibration-proof, protected against corrosion, and are resistant to fuel, oil and grease, reducing maintenance costs.

### ■ Utility I.C.C. bumper



Meets D.O.T. and Transport Canada safety requirements. The 7" deep horizontal bumper provides superior strength and protection.

### ■ Lightweight steel jacketed brake drum



Light weight design features a composite steel shell and centrifuge cast braking surface to minimize costs and increase payload.

### ■ Rear corners designed for high impact loads



The rear corners are engineered to distribute dock impact into the side rail of the trailer for longer rear life.

### ■ Aluminum leg bracing



Aluminum tube leg bracing reduces weight without compromising strength.